

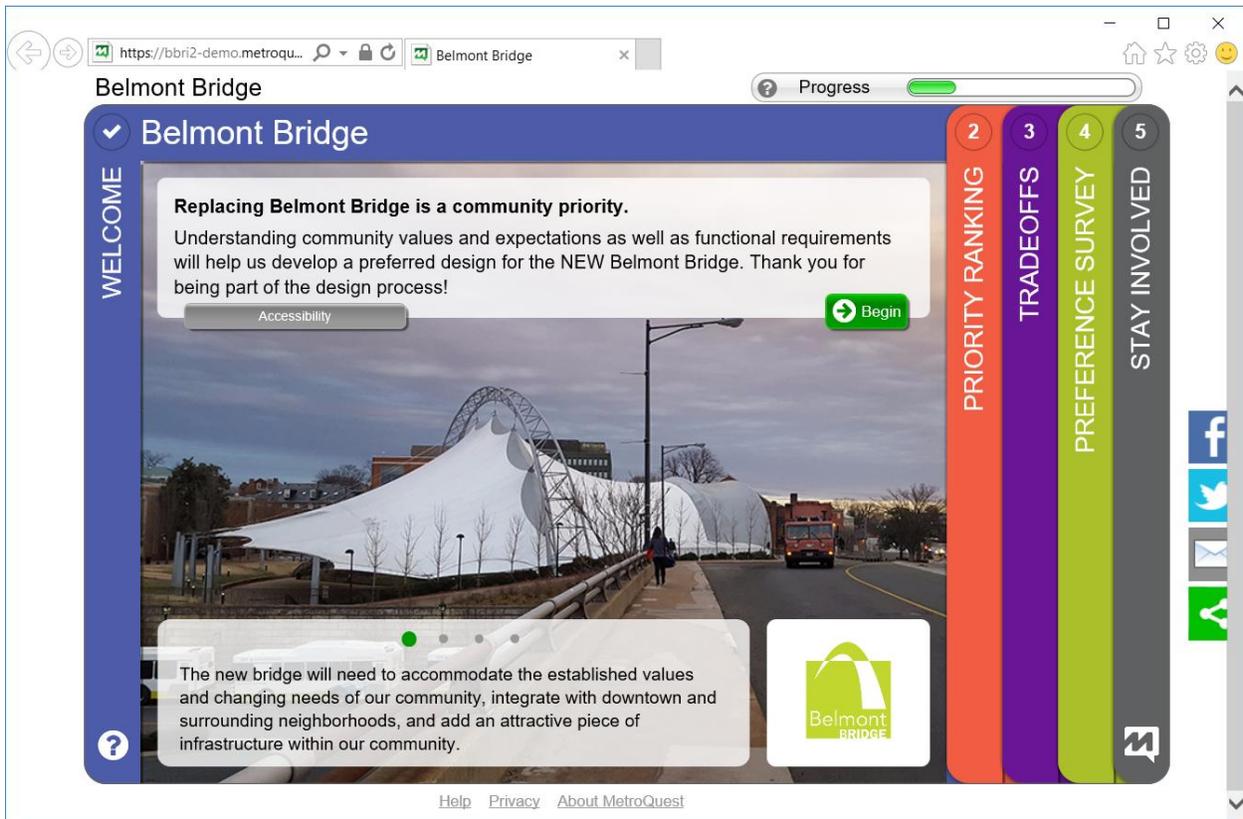
# METROQUEST – CATALOG OF COMMENTS



The Belmont Bridge MetroQuest survey was active from March 11, 2017 to April 16, 2017. A total of 896 participants provided more than 27,677 data points and 771 written comments. The document provides an unedited list of comments for the MetroQuest survey.

Comments are organized according to the five screens presented in the survey.

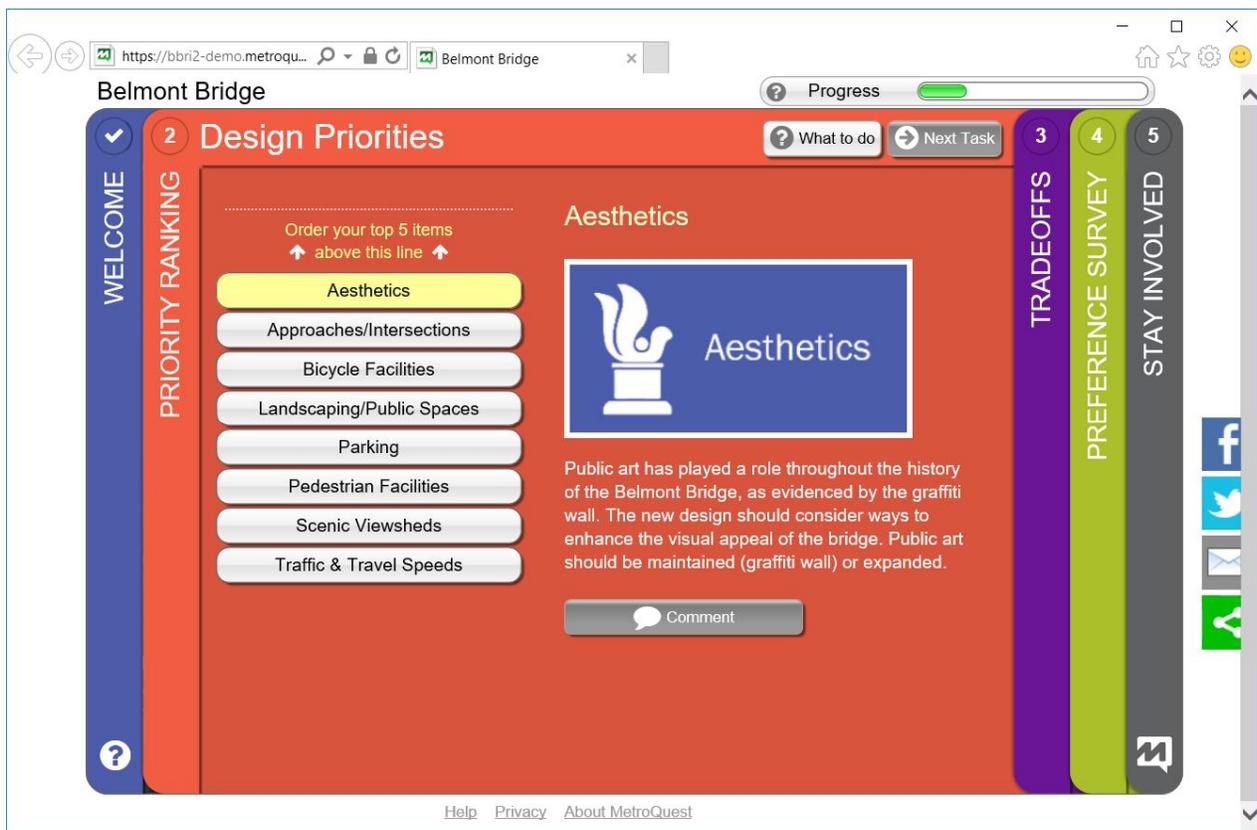
## Screen 1 – Welcome



## Comments

This is an informational screen and no data or comments are collected.

## Screen 2 – Priority Ranking



## Comments

*Aesthetics*

- when I was at the march 11 pavilion event, definitions were not given; now taking the survey, I realize that "scenic viewsheds" (which I didn't understand and didn't vote for) are much more important to me than "aesthetics" which is basically community art.
- Graffiti is a violation of the law. If the City wants to maintain a graffiti wall and do away with historic art in place, like Gens. Lee and Jackson, the comments for this bridge are moot.
- Public art is not a priority for me. But would be great for bridge itself to be a good example of taut, modern, efficient design.
- "aesthetics" is not a separate category to be laid on this like sauce. WTF
- As I see it there are only a few ways to get to Belmont area. Convenience and accessibility are the most important.....and speed and least amount of inconvenience too.
- In lieu of an expensive "world class design" – build a functional bridge to address current and future use.
- I realize this and the pedestrian crossover are the real drivers behind this construction. However, the focus should be on the existing aesthetics downtown... not the method to get downtown.
- I think aesthetics are important, but not necessarily the graffiti wall.

- No need to keep the graffiti
- Please design the underside of the bridge as well as the top of the bridge. No need for a parking lot.
- YES! Let's not lose what makes us unique and creative and fun.
- Preserve or expand the graffiti wall.
- make this an attractive and safe space
- Anything will be better than what is there now. Don't overthink it. We don't have to have a piece of modern art.
- Keep the graffiti wall. Skip the douchy art-in-place.
- Sure ok, not a big priority though. steering question to me
- The bridge needs to accommodate increased vehicular traffic.
- Wide sidewalks is what we want along with the full span and the good views. Dont try for something amazing just do what is there with better sidewalks.
- The graffiti wall is fine, but for me not the focus of aesthetic enhancement. Would rather have the aesthetic considerations focus on architectural design and sculptural enhancements.

### *Approaches/Intersections*

- Unless the City is going to spend major dollars to hide the lobster trap with a design worthy of The Monticello Mountain Bridge, it is just a bridge
- Keep traffic flowing. Congestion is only going to get worse in the future, please accommodate for it now. Cville traffic flow is terribly inefficient. Do everything you can to improve it. We need less stop & go/ better flow.
- Plan for the future! Add the width necessary to accommodate future expansion of traffic
- Please do not forget that the bridge needs to move cars too! Balance that goal with pedestrian and bike facilities, but keep traffic moving!
- Close off Old Avon at the Levy/Garrett intersection. If the left turn onto Levy Avenue is preserved, get rid of the green arrow and open it up to left turn on green. Repaint the turn lane arrows from Garrett onto Avon--put the straight arrow in the right lane, instead of with the left turn arrow.
- Well, I am a little show on the uptake but I am only now realizing that the bridge will have only one land in each direction! So, I think it will develop into even more of a bottleneck than it now is. Has anybody looked at it in the peak travel times? Coming in in the morning, the line can stretch way out of the city limits.
- Yes see comment on traffic
- It should be a 4 lane bridge, to accommodate both existing and future traffic. It is one of the main entrances to downtown.
- Pedestrian and traffic most important
- Don't overthink it. We don't need snazzy strange intersections like a cross-over (Zion X-roads) or roundabouts. Obviously not applicable, but you know what I mean.
- Urban form/ redevelopment potential would be my highest priority, if it were an option. We need new buildings along the sw side of the bridge and relocate the side streets

### *Bicycle Facilities*

- Less cars, and more pedestrian and bike friendly please.
- Bicycle and pedestrians need more protection than the cars
- Building only two traffic lanes is incredibly short sighted. Taking away the parking under the bridge without replacing it is also a bad idea.
- Safe biking is a major concern for me
- Use ramp access to the bridge vs. stairs. Makes it easier to get across the bridge from, say the Pavilion to Champions.
- I am a seasoned cyclist, and I currently avoid the Belmont Bridge because of the lack of safe cycling space. This should be a key priority for the bridge.
- Please insure that cyclists are physically separated from autos.
- Definitely ! Discrete bike lanes are needed
- We need to make cycling a safe form of daily transportation. This also lowers the need for parking. I would use my bike for transportation a lot more if the roads accommodated their presence. Please promote sharing the road!
- The bridge right now is dangerous to ride. There will be many more bikers once there are adequate facilities. Please have ramps down to Water Street.
- Need safe bicycle lanes for travel in both directions, separated from car traffic.
- You don't need an entire lane devoted to biking like it was repainted going south recently. Maybe a shared pedestrian/bike path on one side. Commuters will be riding in the road with traffic anyway since there is not a dedicated bike lane before or after the bridge.
- bikes need to share bridge with vehicles to get 4 traffic lanes
- We don't need that crap

### *Landscaping/Public Spaces*

- There absolutely MUST be something done to stop the noise from the Pavilion. One obvious answer is to plant larger, non-deciduous trees near to the tent, and then add noise blocking elements to the bridge.
- A place to stop, gaze, and sit would be great mid-span
- I wouldn't waste space by putting benches on the bridge...
- Current bridge cannot handle NB traffic flows in the morning or SB traffic flows in the evening.
- I would like to see the area underneath used more for public activities, parks, etc.
- Please preserve the parking lot.
- Please integrate the bridge into our neighborhoods on both ends, and include connectivity down to Water Street. Design the underside of the bridge as well as the top side.
- We need an inexpensive landscape architect firm to come up with the design
- The bridge needs to be attractive with places to sit or converse.

- It already is adjacent to the amphitheater so just make sure it has an entrance to there like it does now and you'll be fine.
- Needs to have at least the same traffic carrying capacity
- The City of Charlottesville has somehow not had leadership that understands the importance of ecological design. The landscape beds around town do not embrace this. They are replanted yearly with plants that have little or no ecological function. This wastes resources of having to replant and further creates disconnects in our landscape. Planting materials that support pollinators and caterpillars, thus supporting birds and beautifying our City. The landscape and Public Place should achieve to incorporate humans as well as other ecosystem function.
- Design must have 2 vehicle lanes in both directions.

### *Parking*

- If there is a way to expand parking that would be ideal.
- The need for parking will always be needed. Also
- the speed control
- With downtown Charlottesville's limited parking, it would honestly be a crime to get rid of any current spaces.
- The fact there is limited parking already should be self-evident. Why make it worse?
- expand other parking areas or ban cars from downtown
- Parking is not in any way an appropriate function for a bridge. We have more than enough free car parking downtown already. The space could be used for much more valuable purposes.
- If you build it (for cars) they (more cars) will come. This is and always will be a pedestrian corridor. Cars and pedestrians don't mix. Prioritize pedestrians.
- Yes, parking is a true need for the downtown to grow and business to grow!
- As someone that works late hours downtown, this is very important. Most of the paid parking garages are closed by the time I get out of work, and that just isn't an option. Free available parking is very important!
- The underside of the bridge should be designed to be a welcoming space--not a parking lot.
- The City is NOT doing enough to provide parking for all of the development that is happening. I live on Belmont Ave and am affected by all of the new restaurants. I can't park in front of my house on the weekends because of all of the restaurant visitors. Any loss of parking would be a negative and the City should do a better job of increasing parking to accompany the increase of restaurants and shops to the Belmont area.
- Keep some parking, lose some parking. Does it matter if they are putting in a new garage just on the north side of the bridge?
- need to keep it
- It was my understanding the City is tearing down low-income landmarks: Lucky 7 and Guadalajara to put up a parking lot. If the correct design is chosen, there will be a number of opportunities other than parking cars under the bridge.
- As a city employee, this is one of the only free places I can park that is close to work.

- We must keep parking a top priority!
- Parking for city employees is already insufficient at city hall, it will only get worse during construction. Part of this process should include a means to improve parking conditions, and this doesn't include taking a shuttle or bus from a parking lot.
- The parking under/near the bridge must be kept. Parking is already a nightmare downtown. Taking this away would be foolish.

### *Pedestrian Facilities*

- Concerned regarding the future of the Belmont Bridge. I think it'd be a much more attractive destination if less cars and car travel were condensed at that location and nearby. Therefore, imho, pedestrians and bike traffic should be the friendliest option. Besides, they may knock down Lucky Seven and Guadalajara for more car access/parking anyway, which may be unfortunate regarding downtown Charlottesville's appeal. Pedestrian traffic need not be a priority while under construction, but for the aftermath, please make the pedestrians a priority. Even if one day I was able to travel by car, I'd still opt for less cars and more walking/running/and biking. We already knocked down way too many trees in Belmont. Charlottesville has to keep in mind future generations and not pollution.
- I am 13 years old and I walk to school over the bridge at least 4-5 times a week. I would like to feel safe walking by myself and not having to worry about cars.
- The new bridge needs to invite people to walk across it, or we will risk potentially deadly crossings of the railroad tracks.
- Footbridge only. No vehicular span.
- Pedestrian safety should be a priority, but I don't believe there should be areas on the bridge for folks to loiter.
- Wide sidewalks, ramp access (strollers, wheel chairs) and the ability to see through a railing to watch the trains pass would be cool for the kids!
- This should always be the number one priority near the downtown mall.
- We live in Lake Reynovia so being able to access the Belmont Bridge via walking to downtown is important to us
- Wide sidewalks on both sides with clear crossing areas delineated please
- I assume these are only local residences. Is there a free trolley that serves Belmont/Downtown? That might be nice on weekends especially.
- Kids love walking across the bridge and watching the trains. Please include a small punchout viewing area on each side for trainspotting. No high fencing or barriers are needed. Just slightly higher than they are now. Thanks!
- Also need to consider better access to Water Street from the bridge.
- We live in Belmont so that we can walk as much as possible. We have a 3 and 5 year old. Right now it feels very unsafe to walk. Traffic is horrible during peak rush hour times. It is not safe for pedestrians, cyclists, and vulnerable populations. There needs to be a clear barrier between CAR traffic and everything else. Not having to deal with traffic lights (a pedestrian bridge?) would be a bonus. The bridge should connect downtown to Belmont in a more seamless fashion than it is now.
- It is very important to have sidewalks on both sides of the bridge and with space between the sidewalk and the car travel lanes.

- You need to have working sidewalks. Simple. We've been without one side for too long.
- We are about to experience a huge loss in parking downtown. there are other ways to do this project without taking away these parking spots
- need to be divided from vehicles

### *Scenic Viewsheds*

- Loved the idea of a raised pedestrian bridge high above the auto traffic in a previous design suggestion. Is it still possible?
- Put the cars on the ground, leave the bridge for pedestrians and cyclists.
- Agreed if it is at all feasible
- The Belmont Bridge is the best place to watch a sunset in Charlottesville. Let's keep it that way please!
- Preserve the sense of downshifting between the business and residential districts as you travel North to South. Preserve the stunning sunset views over the Beck Cohen barn to Dudley Mountain. Please include wider-sidewalk areas for trainspotting. Please avoid an unnecessary tall no-throw barrier/fencing that would destroy the view and create a feeling of being caged.
- Views of the western mountains should be preserved also.
- Yes please don't make an ugly bridge. Let's remember the natural environment.
- There are great views from the bridge, these need to be enhanced.
- I don't really know how you would lose any of the viewsheds with potential designs, but ok.
- I think it is strange that a design cannot be found to address all of these issues listed here. I believe a quick review of how other cities have handled modern bridges in the 21st century would be informative. Belmont is the Brooklyn of Charlottesville, after all, albeit on a different scale.
- I think one important aspects of Scenic Viewsheds is the Architecture of the bridge. Charlottesville is a tourist destination and the bridge has the potential to be part of that economic engine, as it is visible from other parts of the city. Whether it is decided to be rebuilt as the sterile current design, echo the architecture of the pavilion or new transit center, a forward looking design is needed. The City of Charlottesville needs to decide the legacy they want to leave like Chicago and New York have done with their architecture. My view is we look at contemporary design as we move forward. Donald MacDonald had this vision when designing the Ravenel Bridge connecting Mount Pleasant to Charleston SC as well as other bridges listed here: <http://www.donaldmacdonaldarchitects.com/projects/bridges.php>

### *Traffic & Travel Speeds*

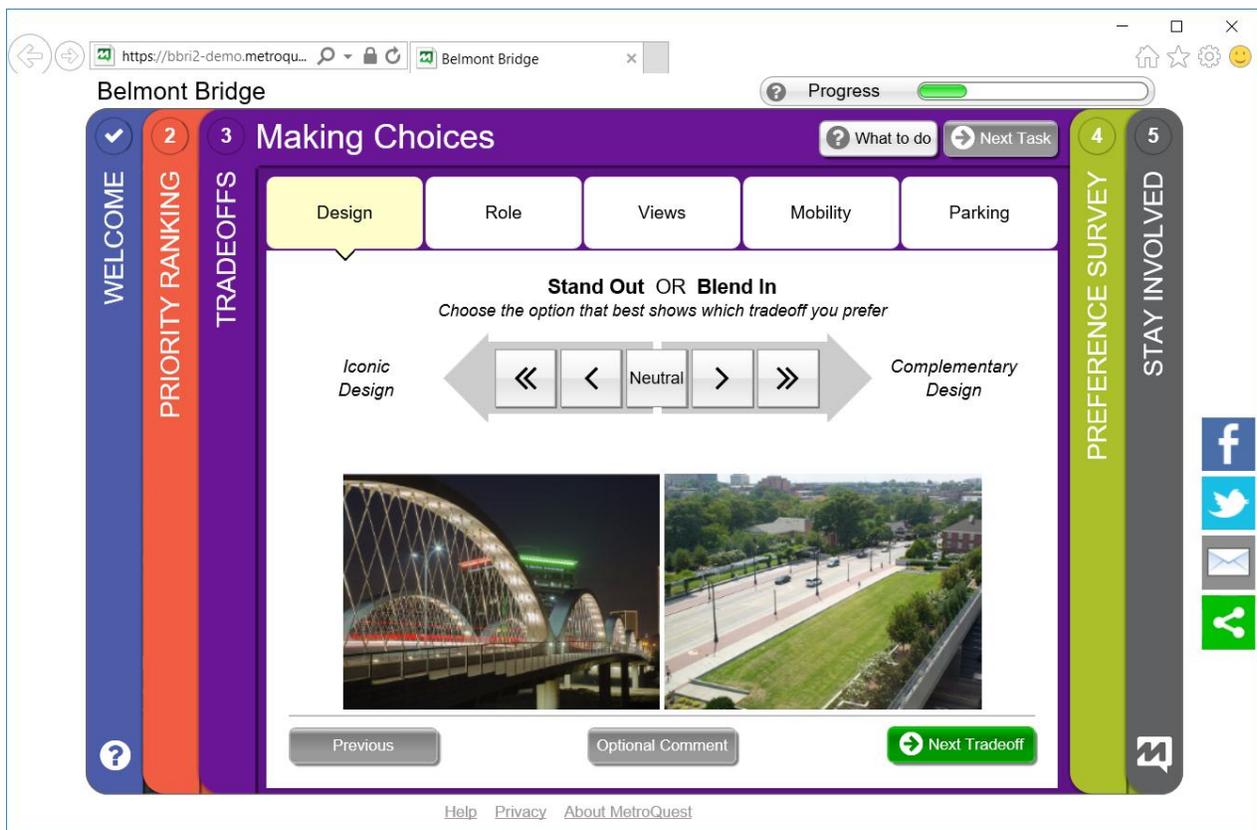
- At its core, this bridge should be able to handle traffic (vehicular, bicycle, pedestrian) for the next 50 years. I don't have access to exact traffic counts, but I'm sure that the traffic volume today is far greater than it was years ago, and it will be much greater 50 years from now. My guess is that motor vehicle traffic across the bridge today exceeds bicycle and pedestrian traffic by 10-1 (just a guess). Even if we are successful in encouraging alternative transportation modes, in the future, motor vehicle traffic will still far outweigh other forms. Don't be shortsighted by creating a bottleneck on/near the bridge by going overboard in limiting traffic volume/speed.
- is it really going to be a two lane car bridge? Are we moving forward or what? Seems to be Cville's way ooling things, let's not forget the Parkway that took 40 years to complete and look at the traffic on that road! It could have easily been 2 lanes each way with little effect on the surrounding landscape, or so it

appears. Now traffic sits coming into Cville from the parkway as it does on Pantops (with 2 lanes of traffic heading west) at 5 p.m.

- Seems very shortsighted and neglectful of those who live downtown and get to have idling cars share their CO2 emissions with downtown neighborhoods.
- There should be least 2 lanes in each direction as C'ville is hemmed in by inadequate road flow.
- I actually think that's the it's core' bikes and pedestrians should govern. With traffic lights at both sides, traffic @25mph is kind of natural.
- The functional aspects concerning traffic, travel speeds, approaches/intersections should be a given--not part of design priorities.
- This is an important road into the City and needs to be able to handle current and future traffic at adequate speeds.
- Isn't this the priority?
- There absolutely needs to be multiple lanes for this new bridge design. As is, coming into/leaving out of town during commuting hours for those of us that live south of town is horrendous.
- Why are we reducing traffic speed? What is your malfunction? The purpose of driving a vehicle is its efficiency in getting from point A to B. Why are you negating its purpose before considering it? You can accommodate all means of traffic if you put your heads together... If you insist on adding more traffic lights, use only Insync through Rhythm Engineering - and add them all throughout the City - If Cville is for lovers, quit adding to road rage. Keep traffic flowing! Consider future growth. Slow & safe are not one in the same. Just separate the various forms of traffic - it's not difficult.
- The bridge is for traffic, cars, bikes, people, and their dogs.
- Make the speed limit compatible to the neighborhoods... Don't let the road and bridge traffic speed be higher than the surrounding neighborhoods
- If you want thriving walkable neighborhoods, you have to force vehicle traffic onto the ring roads. Making it easy for cars always makes it harder for pedestrians and bicyclists. Would we rather be Copenhagen or Los Angeles <http://www.businessinsider.com/cities-going-car-free-2016-8/#mexico-city-hopes-to-ban-about-two-million-cars-from-the-city-center-9>
- I would say 2 way access at all times. Shutting the lanes down will be awful
- Agree
- Are there other options for moving volume? Otherwise, separate cars/trucks from pedestrians/bikes.
- We hope you will spend time observing traffic patterns during the morning and afternoon rush hour. It gets ridiculous. Cars have a hard time respecting the pedestrian crosswalks at any hour of the day. Speed limits should remain very low while in the residential areas.
- In the end we just need a bridge that works.
- needs to be 4 vehicle lanes
- Keep speeds low by creating visual constraints for drivers but create safety for pedestrians. NO straight wide lanes.
- Traffic currently bottlenecks on the bridge. 4 lanes of traffic would also be better for emergency response.
- Needs to be 4 lanes to better handle traffic and future growth

- It is my understanding that the city intends only to build 2 vehicle traffic lanes rather than four which is so desperately needed as has been the case for over forty years. I and or my family have owned homes and commercial property here in Belmont for over eighty years and two vehicle lanes would be a travesty to the community and many people whom pass through the area.
- The proposed building of only 2 vehicle traffic lanes with additional biking & pedestrian lanes is unreasonable. We've seen emergency vehicles struggling to get through for emergencies with the restricted use which was supposed to be temporary only.
- Please do not fail to again build four dedicated vehicle lane.
- Excessive travel speeds? Are you kidding? it's often backed up significantly at peak hours with traffic at a standstill. How will the design improve traffic flow?
- Where is functionality on the list?

## Screen 3 – Tradeoffs



## Comments

*Announcing Arrival OR Connecting Place*

- The design needs to be functional/efficient in carrying traffic
- Connecting places
- Connecting places
- Connecting is more important than announcing
- Connections are more important.
- Bridge would be connecting appealing gathering/retail/residential spaces on both sides of the bridge. No need for flashy signage--its thoughtful design in itself would signal that both sides are equally great destinations for different reasons.
- Let the bridge be another access to downtown; make it user friendly
- We are not Pittsburgh. This is a bridge that crosses a railroad track and connects Belmont to downtown. Making the underside pretty will only please the railroad workers.
- The pavilion already creates a gateway. These should not compete.
- Views to the Pavilion function as announcement of arrival

- Some kind of marker to announce you are on a bridge and entering a new area, but not the showy arches in the photo or flags or anything too visually distracting.
- I am not certain of the difference of gateway vs connection but I think Belmont as a neighborhood should be honored in an appropriate way to reflect it's "blue collar" origins and not make it something that residents do not want. - Flashy or showy.
- Downtown and Belmont are so connected. There is nothing to announce.
- this is not significant gateway to charlottesville, mainly commuter route
- Not even sure how this is a trade off. You can't put a big sign over a shared green space? Not that I want a sign, but it's just a poorly worded question.
- needs to connect and efficiently move traffic
- There are current few other announcements as you traverse from one neighborhood to the next within Charlottesville.
- None, They know where they are by now even if from out of state.
- Both important

*See the Area OR See the Bridge*

- Again, there needs to be noise stopping construction to the bridge. For that reason, the bridge would be more visible.
- See the area
- Both important- views from the bridge resonates more
- See the area
- The bridge can accommodate both- the goals are not at odds
- I support views from the bridge.
- Don't have to see entire length of bridge. But, should be apparent that pedestrians/bikes/cars can travel back and forth easily and safely.
- Really should not be a concern.
- Ideally, I would like the RR Bridge to travel over the road. Similar to the RR Bridge on Rosey Brown Blvd.
- Can't we have both?
- Preserve both to Carter's Mountain and of the sunset.
- It depends on the design of the bridge - if it is "iconic," the view OF it should be important. If it is designed to blend in, the view FROM the bridge should be far more important.
- I enjoy the views from the current bridge and would like to keep these, but the current bridge itself is not attractive and should be improved. Both views are important.
- I'm not sure why this is a trade off? Both of these options are important to me. I enjoy the view from the current bridge and feel strongly that the replacement bridge should be aesthetically pleasing. Both views are important.
- Please preserve the mountain views, and views of the train tracks. No high fencing or guardrails that won't let the kids enjoy the views. No buildings on the approaches to the bridge.

- Well this is hard as we want an attractive structure but the idea is to blend in to the area so it does not become the structure that separates rather than connects. It would be great if there were a bump out on the sidewalk where folks can stand and look west at the mountains and sunset.
- The bridge should integrate into the landscape and its surroundings
- I don't really see how this is a trade off. Both of these will be happening at the same time, your "take" on it will depend on whether you are going over or under the bridge.
- Safety rules here. Most vehicles have only the driver so they should be watching for hazards
- Just like the last question, this is not a trade off. There is no reason to think that a view of the bridge would/should prevent views from the bridge.
- can't really see the bridge because of where it is anyway
- This one makes no sense. I would rather have a bridge that did not obscure the view and blended in.
- Neither matters; get it built and don't spend funds on cosmetics
- This should not be listed as a trade off. Proper design will achieve both.
- EASY! People love a view. The like convenient elevation to 'see the downtown'. And, also to see Monticello mountain, etc. AND there are frequently gorgeous cloud scapes, sunrises and sunsets from the current elevation of the bridge. The City could design to enhance strolling, viewing from the bridge, including perhaps landscapes areas for gathering or sitting at ELEVATION.
- View of bridge very important

### *Space for Parking OR Space for Amenities*

- Install a tiny playground
- get rid of half the parking and make a playground
- Space for amenities nudges out space for parking
- Parking is scarce in the area with all the restaurants and living on Belmont avenue, parking is key for me. I'd like to say amenities, but I HAVE to say parking.
- This is a confusing tab.
- Replace parking so the neighborhood doesn't get the parking.
- A new parking garage is in the works, we should focus on amenities.
- Preserve parking
- You can't take away parking unless you replace it. Too many employees of downtown depend on those spots. Many of them get off work long after the garages close.
- Ammemities
- Make safe well lit public spaces for the community. Make parking garages 24 hours and free during certain hours so there is incentive to park there.
- Charlottesville needs all the parking it can get. Can't we incorporate both?
- My concern is that creating a park will encourage loitering and homeless congregation.
- Not neutral, but balanced. Trade off some parking for some amenities, but not all for one or the other.

- B/c parking is such an issue downtown. If parking issues can be alleviated via other means, then replacing parking is a better option.
- The mall connection to the space in front of LexisNexis provides pedestrian connections/spaces. Water Street under bridge is for cars.
- Considering every single news post about downtown contains nothing but "Yea, what about parking" or "Add more Parking", I think it's pretty common sense that people only care about Parking.
- We don't have enough green spaces for children in this area! Would love to see a playground here!
- We don't have enough green spaces for children in this area! Would love to see a playground here! I also think that the future of our town is NOT cars. It would be a waste to use this space for parking!
- I'm OK with some parking under the bridge, but the landscape around the bridge needs to be greatly improved in general which probably requires removing some parking.
- I'm not opposed to keeping some parking around the bridge but the overall area could be greatly improved by landscaping and pedestrian access.
- Please design the underside of the bridge to create an inviting, family-friendly place. We don't need a parking lot under the bridge.
- I think parking can be preserved by re-designing the roads.
- I would prefer green space but because of the GROWING parking problem in "downtown Belmont" there cannot be a loss of parking. The City needs to create more parking either here or elsewhere. But green space is always preferred.
- While I appreciate public green spaces, parking downtown is becoming increasingly difficult, especially with the construction on Water street. This affordable parking is essential to those who work downtown.
- While I appreciate public green spaces, and would like to see more of them in town, parking downtown is becoming increasingly difficult, especially with the construction on Water street. This affordable parking is essential to those who work downtown.
- I park here everyday, but it's ugly. parking keeps getting displaced with other building projects downtown and I can't afford to pay for parking for my job. maybe you could convert other 2 hour spots to all day spots.
- While the green space would be beautiful, parking is more of a necessity as it is already in short supply.
- This becomes a bit of a moot point when the new parking garage on the corner of Market Street is built.
- not enough free parking downtown now as it is
- The City of Charlottesville needs a Comprehensive parking plan before this can be decided.
- Lowest cost best use under the bridge is for parking as it always has been. You've already created enough other areas for the undesirables to gather.
- Both are important, especially as downtown parking needs increase with our expanding construction of condos and, dare we hope, a hotel?
- City has neglected adequate, affordable parking FOR DECADES. THAT IS the CITY's own stupidity and disrespect for residents, visitors AND WORKERS! The 'happy Socialists' of Charlottesville have actually behaved more like 'affluent aesthetic elites' (a devolved form of centuries-long White patriarchal supremacy!). SHAME! Make the approaches to the bridge PRIMARILY for the community and solve the parking problems in other ways! We call SHAME on the CITY and DEMAND that the City fix it ...

somehow else. Create more quick, easy, frequent public transit - trolleys, etc - in and out of downtown - from Free Bridge! from south of 64 on Avon, from south of 64 on Rt 20, etc.

- If parking isn't preserved, the City will need to create additional parking nearby to accommodate those who park under the bridge.
- An attractive design would be great, but nothing like the ugly Pavilion. That is an eyesore. Shorten the span. Focus needs to be on traffic.
- Replacing parking must be paired with better pedestrian and public parking and sport infrastructure
- Need to maximize both. A good design should not make these two variables a trade off
- parking shortage has been chronic downtown. municipal and store employees could park here and walk; healthy for them, more parking close to businesses left for customers.

### *Stand Out OR Blend In*

- I like both ways
- Costs will probably preclude a design that stands out to a great degree.
- Not clear how to choose -- but my choice is complementary design.
- Blend in
- Blend in is my priority but I cannot understand how to use the up and down buttons
- For att hard to understand. Should be able to select what picture you want. Why are they up and down arrows. Very confusing
- PLEASE don't go with a fake brick/columns just to blend in. It's clearly fake and you end up with something akin to UVA's South Lawn. Hideous.
- I believe the bridge should look good but that the connection is more important.
- I think if it works well, especially for peds and bikes, that even a utilitarian visual design, will be very well received
- Fine with complementary design. Whatever the case, space under the bridge must be a safe appealing place, even if for very utilitarian purposes, such as parking.
- While an interesting or iconic design would be ideal, budget constraints would likely prevent this. The design process should not be allowed to increase the time to construction.
- In general I did not understand this section. It was unclear whether I was selecting what I wanted or what I was against.
- If given a choice make the design simple and periodic. Not like the mess on the left
- I love the natural look of the bridge at Monticello. It makes a statement, yet doesn't look too modern or out of place.
- Iconic, not traditional / complementary. However, iconic does not need to translate to highly visible from a far. My preference is for The Highline in NYC (with cars), not the Brooklyn Bridge or Roosevelt Bridge (in NYC).
- You aren't driving into Charleston, SC or Manhattan. You're driving into Charlottesville. Compliment the existing aesthetics.
- It would be great to have a UVa architect design the bridge, especially if it will have an "Iconic" design.

- Good design will emphasize the buildings and landscape around the bridge. But I still want the design to be attractive. This doesn't mean form and design are not important. It's the small details that can still make this a beautiful structure.
- A complementary design can emphasize our visual culture: the surrounding buildings and landscape. An aesthetically pleasing bridge design can be accomplished through the use of materials and attention to details.
- I want it to be clear that you are crossing a bridge with stunning views. I do not want it to feel like I'm walking down the Downtown Mall with buildings fronting both sides of the bridge. I want to feel a clear pause between the commercial of downtown and the residential of Belmont. With views unobstructed by high railings or fencing, the bridge can both blend in and be iconic.
- The Pavilion is the iconic design in the area. I feel if it can complement the Pavilion, rather than compete, then that would be preferred.
- The option on the right does not provide enough barrier between cars and pedestrians.
- I liked the previous design where the pedestrians and cyclists got to go over and the cars went below grade.
- needs to be cost effective
- These options and the way the indicators are set up is quite confusing.
- Belmont bridge should be a charlottesville icon
- Coal exports are crashing and trains lose efficiency as cars turn autonomous so we can do without a flashy bridge
- We really just need a bridge, not art, and not pretending it isn't there. Just something that gets people over the train tracks.
- just needs to function and blend in. needs to have 4 traffic lanes
- This is a road and transportation project not and monument to the City planners
- City has left the bridge FALLING APART for decades! They knew it was UNSAFE and left it UNSAFE for years, including closing the walk on one side. AND leaving the one open side falling apart FOR YEARS, creating considerable hazard for usual pedestrians not to mention pedestrians with infant carriers, wheelchairs, walkers or canes, etc and pedestrians who are blind, vision impaired, cognitively impaired, etc. CITY should be ashamed, and probably should have been SUED, for its disrespect and failure to equally protect all pedestrians.
- Stand out is more important

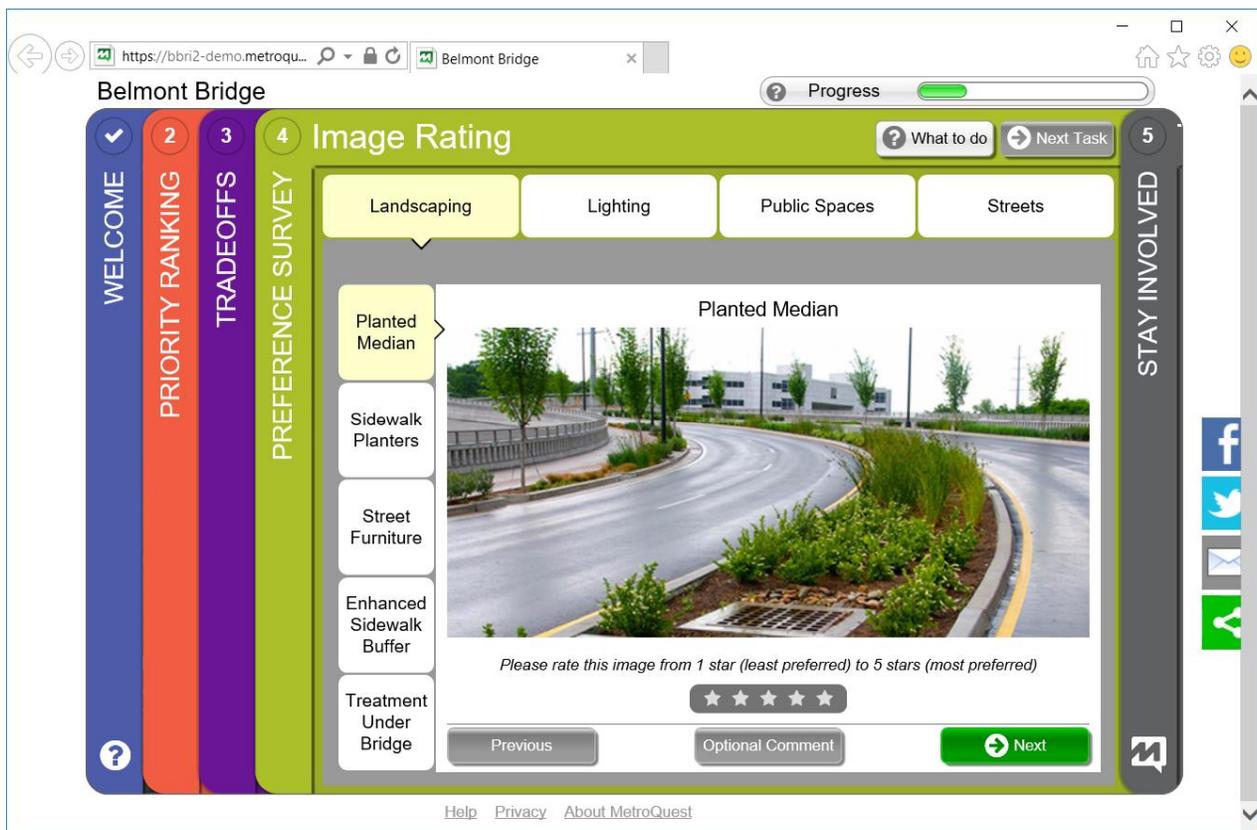
### *Travel Lanes OR People Space*

- I frequently walk on the Belmont bridge with my two young children. It's a little disconcerting with the sidewalk so close to the travel lane. I would love for pedestrians to be separated from the cars. It would be even nicer if pedestrians could come onto the mall at a different point, because we currently have to go a different way when there are pavilion events that put up barricades.
- both are extremely important
- Tough choice, so I'm saying neutral
- I walk the bridge 3 times a week or more, so people very important, but when I'm backed up in 15 cars trying to cross, I know moving cars is key too

- Moving people
- The emphasis should be on pedestrians and cyclists, cars last.
- People space
- BOTH, FOR CRYING OUT LOUD!
- I ultimately think moving people is more important, but I also believe that making it difficult for cars to pass through will cause more traffic in an already-congested area of town, which will ultimately disincentivize pedestrian usage. No one wants to walk through an area of grid-locked cars, breathing exhaust. These two desires should be balanced as much as it is possible, in order to ensure both smooth flow of traffic, and good pedestrian access.
- This is a critical entry and exit point for patrons supporting downtown businesses. We simply cannot lose travel lanes. Too many of our patrons drive to come downtown and spend money.
- People space
- It's important to accommodate pedestrians and bikes safely. Designated spaces and safe crossings for traffic and peds are essential.
- Bikes,runners,walkers, we all need travel lanes
- I would prioritize pedestrians, as long as the traffic on the bridge does not become any worse than it is right now.
- If people space is 'protected', then hopefully travel speeds won't need to be diminished and cause congestion.
- It is imperative that Charlottesville incorporate safe pedestrian and bike lanes into new infrastructure projects. This will only enhance the community.
- More people will cross the bridge in vehicles than by foot or bicycle. Should accommodate all, but vehicular traffic has priority.
- Please de-emphasize moving cars and emphasize moving people. Very important to me.
- It needs to do both of these. Pedestrians and traffic need flow well.
- It needs to do both of these things. It should move traffic easily but separate from pedestrians.
- Protect the pedestrian areas from traffic with a barrier, so families with kids can cross without fear. Separate bike facilities from traffic and pedestrians.
- Both are very important.
- Timing traffic lights so that traffic flows more smoothly could alleviate the need for additional travel lanes.
- Moving people - but traffic is a huge issue. There needs to be clear separation between the two but the City should promote walkability through the design of this bridge.
- i walk this bridge twice a day. i am really afraid of a car veering off and hitting me. there should be a buffer to keep pedestrians safe and feeling secure
- I would like to see a structure that supports and encourages modes of transportation which are more sustainable environmentally. This will make a statement about our local values.

- The bridge currently has 4 travel lanes and 2 sidewalks (although all not currently functional) of adequate size to accommodate all types of traffic - vehicular, pedestrian and cyclist. Not sure how you can justify eliminate two vehicular lanes in your design.
- In terms of "people space" this is a crappy example. Bikes can use the travel lanes with the cars as they are by definition vehicles. The way this is presented is bikes get their way or pedestrians get shafted. Pedestrians >>>> bikers
- This shouldn't be a question. Do NOT give cars priority for this critical community connection.
- The question is a bit misleading as "Moving Cars" is actually "Moving People" as well, just in a different mode of transportation. If you want to discourage cars then a bigger project is needed to allow people to park further out and be able to bike or take public transportation into the city center.
- must have 4 traffic lanes
- Proper design will achieve both. You have both a rural community form Scottsville and a slightly urban community from Belmont. Plotting the two against each other is a bad idea.
- This is a road way first for vehicles and second because of the rail road track crossing issues for people & bikes
- Bridge needs at least 4 lanes. My experience in Cville has been that the city does not invest in the infrastructure to move cars through quickly. This area will continue to grow, and while we all want more people to walk/bike, the reality is more cars. And it's better for everybody to have cars move through efficiently. Build for the future (4+ lanes).
- Bridge MUST do both; convey traffic AND welcome pedestrians. TO DO THIS the City must work with the County to improve frequency of public transit (perhaps a trolley that ONLY conveys from south of 64 back and forth to Downtown across the bridge).
- Moving people very important
- Simply put, both are needed components; neither can be relegated.
- Pedestrians should have accommodation. People on bicycles don't need that crap.

## Screen 4 – Preference Survey



## Comments

*Landscaping***Planted Median**

- The trees planted on the bridge must absolutely be NON-DESIDUOUS trees that will block the noise from the Pavilion - magnolia trees make the most sense.
- I don't like seeing a drain cover in a median. What the what??
- I don't like seeing a drain cover in a median. What the what??
- Also, it's guaranteed that the more shrubs, grasses, and etc. in a public ROW, the more maintenance people will make them ugly through uninformed maintenance practices.
- I don't like seeing a drain cover in a median. What the what??
- Also, it's guaranteed that the more shrubs, grasses, and etc. in a public ROW, the more maintenance people will make them ugly through uninformed maintenance practices. Oh, and there is not enough room for the trees that are planted here. So for all these reasons, I'm giving this a low score.
- only because it is the largest planting.
- I prefer the enhanced sidewalk buffer. I'd prefer the city spend money for an aesthetically appealing and pedestrian-safety measure, rather than just aesthetics (median).
- Is a median being considered? Or is the intent to show any type of landscaping?

- We don't have the space to waste. We need travel lanes.
- Save the vegetation for the approaches. Plants on the bridge is a maintenance issue.
- Looks pretty, then gets overgrown and full of weeds. I would rather have the space
- Rather than in the median, place between bike/ped routes and cars. Then this would have more stars.
- Star ratings don't appear on my screen; FIVE stars
- Increases maintenance and difficult to maintain.
- Only if well maintained. Native plants mostly. Does the city have the resources to maintain it? Unmaintained landscaping is unattractive and the city doesn't have a good record of maintaining streetscapes.
- If it's well maintained and watered this is very appealing to me. Primarily native species please.
- While this is a nice option, I feel more room for travel is needed in this project.
- Needs to be 2 lanes each way (4 lanes). No space for planting.
- Greenery is good, but then you have to worry about maintaining it and in the median does it cause issues for the city workers to go out and perform that maintenance?
- Planted medians require upkeep and potentially take space needed for protected bike lanes. Normal bike lanes do not work as C'ville does not enforce existing lane usage.
- Median is a waste of valuable road space. Need 4 traffic lanes
- Don't waste money on a planted median on a bridge. Use the money for a good buffer between pedestrians and cars. Nicer walking with a buffer than directly next to noisy, smelly cars.
- Something like this would probably sacrifice an actual bike lane for a sharrow.
- This depicts a divided car street. Cars should be to one side and all other modes to another side.

### Sidewalk Planters

- Planters are OK for the Mall, but not for the bridge. Too fussy for a place that most people are passing through at 30 MPH.
- Planters are OK for the Mall, but not for the bridge. Too fussy for a place that most people are passing through at 30 MPH. And, you should clarify: preferred for what purpose? The bridge?
- On the approach, but not on the bridge.
- Street trees, not urns with flowers.
- I would feel safer, and like the look of, the enhanced sidewalk buffer more than sporadic sidewalk planters as pictured here.
- This could be a nice way to separate the bike lanes. Trade off will be maintenance
- Would prefer planting strips, just not in the median.
- Look beautiful but appear to take up a lot of pedestrian (or other) space. TWO stars.
- Yes, if needed as barriers to traffic. Easier to maintain than planted medians(?) and since more accessible up close, they have greater aesthetic appeal.
- I like planters, but these are kind of weird looking!

- These often get in the way of foot traffic. Belmont bridge is highly active with walkers, runners, bikers, and cars. We should optimize the pedestrian space, and not consume it with large and clunky potted plants.
- This is a nice option that is more space friendly
- No, sidewalk planters will just be something to accumulate urine.
- The sidewalk planters on the downtown mall end up just being a replacement for fire hydrants for dogs.
- just one more thing to maintain

### Street Furniture

- Better, but those benches look really uncomfortable.
- Unless it's nicely landscaped/shaded or it's at a bus stop, VERY FEW people actually use these benches.
- Furniture placement at ends of bridge, but not a priority on the bridge--just not sure how much width is available.
- don't see the need for street furniture
- Something less traditional
- We do not need more places for vagrants to drink and sleep.
- There is better more contemporary furniture out there.
- we could use more benches on the mall!
- The bridge should not be an area for people to sit and catch a free show at the Pavilion. It's a travel route.
- Yes to street furniture. But no to benches as objects. Street furniture should be integrated with buffers, planters, guardrails, etc.
- Unless space is unlimited, a bridge seems like a way to get people from one place to another. Plenty of places to sit down and enjoy the views on the DTM. ONE star.
- Depends on locations. Would be good to space regularly as rest stops for people that need it. Also where view shed warrants.
- This is for the Belmont Bridge right? The Bridge that takes 2 minutes to walk over? No one should be hanging out there on seats and no one should need a rest.
- I'd be concerned about homeless loitering on the bridge. I don't think benches are a good idea from a safety perspective.
- This would be especially nice if we have created a beautiful space with a view and want to encourage foot traffic. It will also extend the experience of the downtown mall.
- While I like benches and furniture, not sure how that applies to bridges. Are you planning on people sitting and enjoying the view from the middle of the bridge?
- why?
- No benches on bridge.
- Why would we clutter a traveling bridge with street furniture?

### Enhanced Sidewalk Buffer

- something substantial so kids don't have to be held tightly while walking
- Interesting approach, but we will be constantly putting money into repairs.
- that is where the street trees should be place
- This is a fantastic idea! I would feel so much safer being near fast-moving cars with such a barrier.
- maybe but this one looks dumb
- I do appreciate buffers because they prevent pedestrians from being splashed by cars when it is raining.
- In concept, yes. The execution in this image is a poor representation of what I hope we get.
- rather have a bicycle lane buffer.
- A lot of extra money to build and maintain - surely there are more cost-efficient ways to separate cars, bikes, and pedestrians?
- Very important to make pedestrians feel safe.
- I like the idea of a planter buffer, but this was is too modern.
- i love this! planters are nice but i imagine costly when they are filled w annuals. use perennials and native plants. sidewalk buffer way better than all these other options
- I like this idea especially if pedestrians are in close proximity to vehicles. As a driver, it feels safer when there is no chance that a pedestrian might step into the street from a crowded sidewalk.
- This would greatly increase safety for a shared bike/pedestrian path to be separate from the car travel lanes. Makes more sense than plants in the median.
- just need a buffer, nothing fancy- needs to have as small a footprint as possible to allow for 4 traffic lanes
- Pedestrian safety is more important with a good buffer than a median between cars for a bridge.
- The stronger the better: separation of pedestrians (and hopefully bikes) from vehicle traffic. Current bridge feels like walking over a vast parking lot.

### Treatment Under Bridge

- One star because I think that parking must be preserved.
- Just OK. I see a nice place for crime.
- The bridge is so close to large downtown spaces, I feel like the space under the bridge is not a priority for me or my family.
- I am not sure that my rating was saved. It does not appear above when I went to review my entry.
- Really believe in replacing parking to an enhanced public amenity/greenspace to create a connection between Belmont and Downtown. Creating a place.
- We need the parking spaces down there.
- Activate this space with art, light, activities.
- Is under the bridge the best place for green space? There may be better opportunities for green space in the future development of the parcels near the base of the bridge that are not currently city owned.

IF there's green space, perhaps it's small and just the tip of the finger of green space that becomes the greenway that's part of the SIA.

- Although care should be taken to evaluate potential negative aspects of a parklike setting in terms of late night crime, drug activity, etc. Regret that this is becoming a more frequent concern downtown and may preclude this approach.
- Obviously, it looks lovely. However, parking is already ridiculous downtown so not in favor of extended greenspace (under a bridge?) to the detriment of parking. ONE star.
- This is extremely appealing compared to what is presently there now. I would also urge designers to keep/replace the graffiti wall if possible. I particularly like the replacement of asphalt with dirt, grass and plants!
- I would like to see treatment under the bridge, but not in place of parking.
- Will encourage bums
- Again, the green space is nice, but parking is more of a necessity.
- While currently used for parking, I am not sure how inviting the space would be to other activities given the active train yard and echoing noises caught under the bridge. It could be part of a more comprehensive bike/pedestrian path like shown in the picture but I doubt the city has the plans for such a feature on that side of the bridge.
- need parking first
- Treat as urban space: think Burnside skate park, Portland OR.
- preserve parking and 2 lanes in both directions
- This will sacrifice parking, which is already insufficient.
- I find this misleading. Yes, treatment under the bridge would be nice but not at the expense of parking. Parking is more important than a pocket park.
- Hard to make sense of this.
- Area beneath bridge is, I feel, important for parking and connections to street grid, but also should be visually welcoming. i.e., not a starkly flat asphalted lot, but one with interspersed small trees or plantings

### *Lighting*

#### **Pedestrian-Scale Lighting**

- OK, but what is that central lane?
- So important for safety as so many people will be walking when it's dark
- I think whatever option provides the best visibility should be chosen. Aesthetics should be secondary to safety when it comes to lighting.
- Keep dark night sky
- too much glare, light sources should not be visible
- This seems the safest - are there statistics about this?
- Definitely!
- Implement lights like those used at the 29/Rio intersection

- Good lighting at night is key! It makes people feel much safer walking alone at night and helps cars with visibility.
- Overhead glare is harsh. The character added by the light-wrapped trees is getting the star.
- FIVE stars.
- Definitely the most important aspect to this project. I've walked that Bridge many times at night and it can be a bit dark. Later in the evening, the sketchier the characters that hang out there.
- Please get rid of the orange tinted street lights. White lights would look much nicer.
- Please get rid of the orange tinted street lights. White lights would look much nicer. Also have the light pointing down to reduce light pollution.
- Please add white light instead of orange
- Not too many lights. ex: The West Main St bridge has far too many light fixtures.
- Not too many lights. ex: The West Main St bridge has far too many light fixtures. Lights should not project upward and disturb our skies.
- Lighting is important if the light is projected downwards or hidden in the structure itself. Not too much as to preserve the night sky.
- Would there be a bridge option without lights?
- all of these example photos are unattractive. light pollution is an increasing problem... lighting should be as minimal as needed to be functional

### Pathway Lighting

- fancy. But could you see faces of people you pass?
- Money money money...Money!
- Sometimes I walk on the bridge alone at night. I feel safer when it's brightly lit.
- there is no image here
- Soft and where it is necessary. Unsure of blue color, but color is nice and would relate to the desire for public art.
- Lighting equals safety...please make it well lit in a multitude of ways.
- ONE star.
- I'm not sure on this one. Let's think about light pollution and energy efficiency as well as safety.
- With a separated walking/biking path this kind of lighting would be nice unless it was drowned out by street lights from Water Street.
- This example does not appear to be bright enough to address safety concerns
- I might rate this higher but not if this is replacing lighting necessary to be safer. This is cool, but not sufficient for safety.

### Over Street Lighting

- Over Street Lighting does nothing to illuminate pedestrian traffic (see Emmet Street, Burnet Commons area). Safety is compromised for what? Lighting should be like the lighting on West Main- pedestrian scale- it also does a fine job of illuminating the street.

- I don't care how high the lights are, but these are ugly.
- cars have headlights, over street lighting is not necessary
- Need to consider dark skys
- This is terrible.
- zero stars
- THREE star.
- Please limit light pollution!
- Not needed.
- I'm starting to get the sense that this survey is getting a little too in the weeds. This level of lighting can only be determined once the function of the bridge is decided.

### Bridge Lighting

- Charlottesville has historically not been into lighting things up. Why start now?
- Lighting should also encourage safety, visibility, gathering (where appropriate).
- depends on how the space below is being used
- Lighting the underside would add to the character.
- ONE star.
- Only as needed for safety underneath.
- I don't really like the green, but the concept of lighting under the bridge could be nice.
- Only needed if the bridge structure is attractive enough to highlight.
- This is interesting and appealing if the area under the bridge were redesigned.
- I'm sure the empty coal trains would love the accent lighting. Just kidding, but really all of these lighting questions are a little over the top.

### Architectural Lighting

- my rating is for this image; there are way better examples of architectural lighting
- To what end is this lighting? Do you plan to clutter the sidewalk with these things?
- Looks great until they are not maintained.
- Lighting should be functional. Letting architects pick out weird fixtures that they think attractive is not an appropriate way to spend public money.
- Looks like a trip hazard. Many runners on the Belmont bridge - I frequently run at night. Also, these look like they'd be in the way of strollers.
- ONE star.
- I can't tell what this is.
- i walk here at dark once every day and as a woman it is very important to feel safe. architectural lighting makes the area seem better kept up and maybe less likely to encourage shady activities. if the

light is for the cars it wont be nearly as pleasant as if its designed for pedestrians. the street lamps on downtown mall are dated 1990s looking; choose something more timeless please

- Do what you want with lights. I don't think people really care when the current bridge barely functions.
- As a professional lighting designer and engineer, this portion of the this survey is, shall we say, not only useless but borders on wacky.
- No context here. Hard to make sense of.

## *Public Spaces*

### **Pathways**

- I prefer authenticity in materials. If it's concrete, make it look like concrete, but be creative with the joint design and the finishes. Don't insult us with fake-y materials. Also, Those little walls are too dinky for a large space. Also, what is going on with the "plop rocks?"
- I like this image because of the number of trees and shrubs beside the pathway, not the pathways in themselves. I desire trees here.
- Urban murals!
- Aren't we discussing a bridge and not another downtown mall...
- too rough for a wheelchair. Attractive but this would highly impact my life negatively
- too rough for a wheelchair. Attractive but this surface would highly impact my life negatively
- However, I prefer pathways, to maximize using the bridge space for travel across it.
- Cool for sidewalks but this is a bridge not a park
- stars did not appear on the screen so am unable to rank; something is wrong with this segment of the survey
- If it's made wide enough, should be able to accommodate cars, bike lane and sidewalk, then maybe potted plants or some visual mural?
- varied materials gets the star
- FIVE stars.
- What has this image to do with a bridge path?
- No brick or similarly uneven paths that are dangerous for those who have balance issues etc.
- While I like the pathway, do you really think there is room for a brick path when you are already talking about planted medians, barriers, benches, and tons of lighting?
- looks really expensive
- Treat as urban space, not suburbs.

### **Plaza**

- Looks like a place to go and be hot in this climate.
- Looks like a place to go and be hot in this climate. I understand what you're doing, though, but I seriously doubt that any contractor in this dinky town could pull it off.

- none of these choices make sense for a bridge, maybe underneath. Would we still be able to see the view from the bridge if these were built?
- This is a bridge not a park
- For what purpose would you need a plaza on a bridge?
- too harsh. different colors may help
- Downtown mall already there
- No one is going to sit on a bridge with traffic flowing nearby and no commerce to attract anyone. This idea is profoundly dumb.
- we already have the downtown mall and amphitheatre to serve this
- Like the colorful paving, but not necessarily the idea that tons of space is dedicated to a large open plaza.
- TWO stars.
- It is a bridge, not a space.
- Be creative in a positive, mindful way.
- The bridge should not be a hangout spot. Marketing it as so allows for the homeless population to sleep there. It is unsafe for them and other citizens.
- I like plazas but not this particular one.
- I like the general idea of this but this color scheme is making me sweat - I can't imagine wanting to linger in this space during a hot VA summer.
- Now I'm really confused. Is this a bridge replacement project or a complete redesign of the downtown mall? You guys really need to re-evaluate your focus.
- Thought we were building a bridge.
- Where? Under the bridge? The photos in this survey are often so far from applicability to the Belmont bridge site that one wonders what the designers are thinking. Urban design fantasies, perhaps?
- I think a plaza on the Belmont Bridge would be silly.

### Active Space

- I'm assuming this is for underneath the bridge
- Not necessary so close to Downtown.
- none of these choices make sense for a bridge, maybe underneath. Would we still be able to see the view from the bridge if these were built?
- This is a bridge not a park
- Not on a bridge
- The Mall and other nearby facilities are the active space that should be in play. Don't try to make a bridge compete with the Downtown Mall.
- In concept active placemaking is preferred to its alternative, but this image conveys it as a plaza again, rather than activated linear space.
- FIVE stars.

- It is a bridge. The Pavilion is next to the bridge. Don't duplicate functions. Reinforce them.
- Create multi-functional spaces!!
- This is going to be a heavily-trafficked bridge. Making it an active or play space, or a parklet, seems unwise.
- Isn't there already an amphitheater that is literally touching the bridge? Let it go and build a bridge.

### Play Space

- phrase onyegrste interesting [lay struyures as is commom in Europe
- There is enough for kids in this town. We need something for grownups.
- I love the idea of a play space as there isn't a park on that side of Belmont (except Clark which isn't really convenient as you are walking back and forth from downtown).
- This is a bridge not a park
- Really, on a bridge?
- It's a bridge, not a freakin' playground. Who wants to take small children to play near a large volume of motor traffic?!
- Space for kids to play is wanted, but not objects like this. Playscapes that look like they are integral to the rest of the bridge finishes would be welcome.
- ONE star.
- It is a bridge
- YES, YES, YES - it has to be family friendly, safe. Otherwise, you miss the point.
- We should absolutely not have kids playing on or near a bridge.
- There isn't much for children within walking distance of downtown Belmont and there are tons of kids. A playground would be great!
- Don't need play space on a bridge
- I'm done with thinking this is serious if you are talking about playgrounds for a bridge replacement project.
- Limited to only people with kids. Discriminatory.
- Dog populations seem to be large in Charlottesville and across the country. More enclosures are needed
- Seriously - a playground on a traffic bridge.....YIKES!
- None of these in this category seem applicable to the bridge

### Parklet

- Just no. The trend for wood doesn't work in Virginia.
- not sure what these represent to you
- I would like the idea of a parklet, but see no trees. Trees put the park in parklet.
- We moved to Charlottesville from the Andersonville neighborhood of Chicago where a number of these 'parklets/people spaces' replaced on street parking. There was one which had comfortable benches

and was located outside of a coffee shop and was a benefit, but all the rest in our neighborhood where more interesting concept than functional space.

- Not necessarily on the bridge, but on one or both sides of it.
- Love parklets. Can be designed by local artists and change periodically
- This is a bridge not a park.
- Are all of these really being considered for the bridge? They seem out of place...
- It's a bridge
- Taking back space from cars is obvious and wanted in concept, but this image is so car-centric, covered in concrete, that the proportion of parklet to street is out of balance with my wants and therefore only gets one star.
- Only add amenities that our community can afford to upkeep: gardening for plants and police officers for patrolling/safety.
- ONE star.
- Its a bridge.
- It's a bridge. Not a park
- The picture shows wooden seats. Not sure how this qualifies as a Parklet. I would assume some planting would be used.
- If the public spaces here are intended to replace the parking, I'm against them entirely.
- I want to drive and walk over it and park under it. Why do we need a parklet?
- I want a bridge that's safe for traffic - vehicular, pedestrian, bicycle. Not one that's trying to be something else like a park or playground. We have plenty of those.

## *Streets*

### **Blended Space (Bike/Ped)**

- No--put the bikes in the street where they belong.
- I don't understand these concepts. Peds and bikes should be in separate lanes but alongside is fine. Bikes should be safe from cars too.
- This seems like it would be safest for bikers and pedestrians, so it would be my preference.
- Even though the starts have not appeared on my screen, I have guessed their positions and have submitted rankings with the 1 on the left and the 5 on the right; hope this is correct
- Tricky to do since bikes aren't allow on sidewalks?
- On the downhill, bikes can really pick up some speed - I don't think you should combine bike and ped on the southbound side. Might be okay on the northbound side.
- I think that bike lanes should be separated from pedestrian space. "Blended" paths usually become pedestrian dominated and the speed differential becomes dangerous or forces cyclists to slow down.
- bikers and pedestrians need to be educated about bikes and traffic. Bikes are part of the traffic flow but many do not act like they are which confuses everyone and becomes dangerous
- Now we are talking!

- These blended spaces can lead to conflicts between cyclists and pedestrians. I cycle in Cville too and it's VERY frustrating when pedestrians use lanes and spaces intended for bikes.
- I'd rather allow bike / ped mixed than reduce the space needed for either, but this image has no markings to indicate the best placement for either and will lead to conflict.
- If bikes are to share space with pedestrians (such as a sidewalk) this must be clearly indicated.
- FIVE star. Frankly, don't know enough about ped/bike safety to make an informed vote. Whatever is SAFEST for pedestrians, then bicyclists, then cars ...
- This should be a top-notch bike commuting facility. Please separate bike/ped from vehicle traffic and one another.
- as a pedestrian this makes me feel the safest. a buffer that could stop a car from hitting me is the single most important feature i would like for this bridge
- No blended space. Bikes are vehicles and should be in the vehicle lanes.
- works for be if it gets 4 traffic lanes

### Single Travel Lanes

- I think separated bike and pedestrian areas would be ideal. Cars, bikes and pedestrians travel at different speeds and have different needs. Separate them to keep everyone safe!
- The trees here will fail because median doesn't not enough soil volume and too close together for big trees. No more dying tree medians. A boulevard would be nice, but median is not wide enough in picture.
- Please maintain multiple lanes that currently exist, or, add a lane to the side that only has one. Do not increase traffic congestion at the bridge.
- What does this mean? Direction of traffic separated by median, or saying that bike and car share the travel lane (not in favor of this).
- This is a capacity issue
- NO NO NO. This will create massive traffic backups.
- Push all cars to the east with single travel lanes and allow bikes and peds to occupy western side. Careful consideration at intersections with turning lanes will be a necessary tradeoff.
- rather have the median space used to protect cyclists from traffic.
- FOUR stars.
- Need to have multiple travel lanes. Too much traffic at peak times with persistent backups.
- Need to maintain separate bike/ped facilities across and beyond bridge in both directions, even if there are turn lanes (unlike current temporary paint job).
- You need to reconsider the backup this project will present down Avon in the mornings and to Market and beyond in evenings. Creating gridlock isn't good for anyone.
- epic fail waiting to happen
- More car lanes!!!!
- Reducing the bridge lanes from 4 to 2 (as seems to have been decided already) is a MISTAKE that should be brought to the attention of Cville City Council.

- Hope this includes separating bikes from pedestrian traffic.

### Separate Space (Bike/Ped)

- No--bikes are vehicles. Let them learn how to ride in traffic like grownups.
- I like separate facilities but the bike lanes should be protected
- Bike is not separated from car, but is separated from ped? Can we separate all - vehicle, bike and ped individually?
- use turn lane space for separation of vehicles from bicycles
- THREE stars.
- I think the options and trade-offs presented here for Bike/Ped are unclear... Bicycles should have a dedicated lane that's also physically protected from vehicular traffic. Put another way -- as a first principle, bicycles should be able to transit the bridge and make turns without constantly worrying about the nightmare of being hit by an inattentive driver. But meeting that principle by forcing bicycles and pedestrians to dodge around each other isn't the answer. Getting the utilitarian transit experience right for each of the three main modes of transit is \*much more important\* for the bridge than other more superficial design elements considered in the survey, such as furniture, landscaping, fancy lighting, and so on.
- Yes to separate spaces for bikes and peds, but would prefer more protection for bikes than shown in the photo.
- Because of velocities, buffered bike lane is better.
- Bike lanes need to be protected!
- not sure there is room to support 4 traffic lanes in this version
- This image looks the same as the Single Travel Lanes image
- Hard to make sense of.

### Multiple Travel Lanes

- No.
- Don't think there is enough room for this and walking and bikes.
- As needed for capacity
- Showing backed up traffic will certainly prompt the answer you want. In any case, we need multiple traffic lanes.
- Build for the future, not for the present. Zero stars = not preferred at all.
- THREE stars.
- i support this if it helps traffic flow. charlottesville has issues with flow. people complain about population here but i dont think roads are designed to optimize flow. you cant be in denial of the city size with this project. if it wont be a car artery then another north south street nearby needs to become it. the way the lanes merge on the bridge now doesnt make sense. restricts flow for no reason
- must have 4 lanes of traffic and improved approaches. keep ped access, make bikes share with cars

### Limited Space (Bike/Ped)

- I have no idea what "limited space" means.
- Not sure what this means—bikes and people share the sidewalk?
- Pictures not entirely clear. I support prioritizing pedestrians and cyclists
- Horrible. Don't do this.
- Awful. (And using this image / location in Cville to illustrate anything is also tainted.)
- What makes some of the worlds best cities awesome?? The ability to transport oneself easily and quickly...think European cities.
- TWO stars.
- The Limited Space (Bike/Ped) photo would be a tragic end-product for the Belmont Bridge, and a stunning loss of opportunity.
- only way i see to get 4 traffic lanes.
- Bikes are a pain in the butt. No bikes !!
- Keep the bridge as narrow as possible.
- I would like to see bike lanes and a separate sidewalk for pedestrians. That's the main point here. And the number of lanes should be limited.

## Screen 5 – Stay Involved

The screenshot shows a web browser window displaying the Belmont Bridge survey. The browser address bar shows the URL <https://bbri2-demo.metroqu...> and the page title is 'Belmont Bridge'. A progress bar at the top indicates the survey is nearly complete. The navigation menu on the left lists five steps: 1. WELCOME, 2. PRIORITY RANKING, 3. TRADEOFFS, 4. PREFERENCE SURVEY, and 5. STAY INVOLVED. The main content area is titled 'Thank you!' and includes the following sections:

- Optional Questions:** A section with the instruction 'Please check all that apply.' and five checkboxes:
  - I live in a neighborhood near the bridge.
  - I work in or near downtown.
  - I walk across the bridge often.
  - I bike across the bridge often.
  - I drive across the bridge often.
- Additional Comments (optional):** A text input field with a placeholder 'Type...' and a dropdown arrow.
- Receive Project Updates?** A dropdown menu with 'Select:' and a downward arrow.
- Email Address (to receive updates):** A text input field with a placeholder 'Type...'.
- Buttons:** A green 'Submit Final Questions' button and a grey 'Skip' button.
- Thank you! Message:** A text box containing: 'Thank you for providing input! Your participation is critical to the success of the process, and we want to stay in touch. Please visit [www.belmontbridge.org](http://www.belmontbridge.org) for more information.'
- Logos:** The University of Virginia seal and the Belmont Bridge logo.

At the bottom of the page, there are links for 'Help', 'Privacy', and 'About MetroQuest'. Social media icons for Facebook, Twitter, and Email are visible on the right side of the screen.

## Comments

- This is a project that we hope will stand for many years, it's important to get this right, and it's worth spending money on.
- thank you for taking our interests into consideration! For me, equal access for pedestrian, bike, and cars is the most important aspect of the new bridge. After that, low-key, integrated aesthetics with plants feels important. Thanks!
- Words that come to mind with this project:
  - \*Respect
  - \*Sustainability
  - \*Outdoor Surroundings and keeping children interested in doing outdoor activities (even though I don't have children - they are the future of Cville)
- If planned with ample protected bike/ped lanes, many more people will opt for these modes and reduce traffic
- Cycling/Pedestrian corridor into town
- Priority must be given to the concerns of residents around the bridge. Everything that happens in that area DIRECTLY affects us, and often in a negative way, i.e. noise from the Pavilion, lack of parking downtown brings more traffic into our neighborhoods, etc. These types of issues, ultimately, adversely affect the values of our property.

- The "gateway" bridge needs to move the cars in an out swiftly or it becomes an eyesore seeing traffic seating stalled on the bridge... no matter how nice it looks.
- I think it is essential for Charlottesville to become more bicycle/pedestrian friendly in order to develop community and start to approach the future of autonomous vehicles.
- I would really like to see inviting, robust pedestrian access. I cross this bridge twice a day, as does my 13 year old daughter. The current bridge is neither inviting nor does it feel safe as a pedestrian. Pedestrians are so much more exposed to everything that it makes sense to make them the priority. When I'm driving across the bridge, it is much less of an intimate experience, so I don't care so much about how it feels.
- The sidewalks need to be wider, no median, and no bike lanes. We don't need plants up there, but vertical furnishings at the edge between car and walker are important. I prefer contemporary furnishings like those wonderful silver light standards. It would be great to walk across the bridge and not feel like one is on display.
- Make the bridge as pedestrian friendly as possible
- Please make bike safety a priority in the project. Cville is home to many cyclists, and a lot of our infrastructure is just abysmal for biking. Let's not pass up this opportunity to design in ways that are bike and pedestrian friendly.
- Although the bridge needs to accommodate pedestrians well, there needs to be consideration for rush hour traffic as it tends to backup on the bridge.
- As a daily bicyclist, I hear many people say they feel Cville is not safe enough for biking. I'm in favor of anything that would change that! Thanks; great study!
- I thought the design was done and have been waiting for construction to begin. Function is more important than design. GET IT DONE.
- Belmont is \*in\* Charlottesville. This bridge is not an "entrance corridor". People like to walk from Belmont to the Mall, and visa-versa. They liked to even more before the railroad put up that damned fence!
- I was born here. It will be shocking if this bridge ever gets built in our lifetime.
- While I understand it has already been passed on, I prefer the tunnel option.
- Imho, Driving or walking to a location that has many cars would not be ideal.
- Don't get too fancy. Prioritize quality, long-lasting materials and construction over aesthetic features. Keep traffic moving and cyclists and pedestrians safe. That's it.
- I wasn't sure I understood the part of the survey where we picked neutral or the arrows. I made a comment to clarify my answer in case I did the survey incorrectly.
- It seems that keeping the bridge useful to all three methods of travel is pretty high on my list. I'd also like to keep Charlottesville an attractive place for a wide variety of people.
- I walk or bike across the bridge 2-4 times per day.
- Four-five years ago we went through all of this with lots of meetings and a design contest. Was that all thrown out the window? What a waste and what a disgrace...
- I was a little confused about rating the various pictures. Was it meant to ask, "If we have a plaza, is this how you would like it to look?" or "How much do you like the idea of having a plaza [generally, whether or not it looks like the particular example, or specifically do you want a plaza that looks like this one?" I

answered more toward the latter but it was a bit hard to rate things for that reasons. But otherwise, nice survey, and I appreciate the opportunity to participate.

- I'd like also to be able to walk my stroller and dog across the bridge w/out fearing rushing traffic.
- This was an extremely poorly designed survey from a survey methodology perspective. Make sure to take all the data collected this way with a huge grain of salt...
- Having a bridge that connects the neighborhood to downtown in a way that is friendly and safe for pedestrians is very important. The construction of a new bridge is a wonderful opportunity to expand all of the positive aspects of the pedestrian mall to Avon St and the surrounding neighborhoods.
- Would walk more often if getting to bridge and walking across it were safer and more appealing.
- I live off of Little High St.
- Would like to see the bridge and surroundings have aesthetic and functional value and should be destination point.
- The bridge should be of a design that we can be stands out and we can be pride of. The big question is where the money going to come from to pay for all the features that we would want.
- Driving across the bridge is the most efficient way for me to get to work from my North Downtown house.
- Please please please make this bridge bike and pedestrian friendly. My family and I use this bridge daily.
- Not sure if Mill Creek qualifies as "near," but I cross this bridge often. I would not want it to become a bottleneck during commutes just so it can be "artsy." Function before form.
- I think the focus as far as traffic should be to encourage increased use of walk/bike, public transport, and ridesharing as much as possible. There are other more appropriate traffic corridors for County residents on the south side than coming over the Belmont Bridge. Movement of larger volume of cars should not be a consideration in this design.
- I live on Hinton Ave and use the bridge every day. I would definitely bike if it was safe to do so, but there's not enough room for walkers and bikers so I choose to drive or walk. Walking is kind of scary, however with no barrier between the cars and pedestrians.
- It's very important to make the bridge pedestrian friendly, I walk to work on it every day and the safety of people (next to cars) is key!
- The perfect bridge for me would have car traffic below and pedestrian/bicycle traffic above. Two tiers. And the above tier would be like the high line in New York.
- Lets be smart and build a cost effective bridge that serves ALL users and that is not an impediment to commerce.
- Walk under the bridge to/from work Mon-Fri
- i think it is very important for this bridge to be well-suited to accommodate ALL modes of transportation well, walkers, bikers and cars.
- No vehicular bridge. Footbridge only. Fire Jeanette.
- build somethong that enhances the beauty of Cville
- I am in a power wheelchair
- Please it really iconic. I am not concerned about the cost.

- Please make it really iconic. I am not concerned about the cost.
- Local know not to drive across Belmont Bridge and this should be used more for pedestrian access to the Belmont community. If you're commuting downtown via car, it's easiest to use Monticello Avenue, East Market Street, or the new connection street at East Water Street.
- Thank you for asking for input.
- While I care about parks, green spaces, etc., what I want from the bridge is a functional, attractive passageway, not a destination. Charlottesville has many quality parks, etc.; let the bridge just be a bridge (and get it built!).
- Thanks for putting this together! I really appreciate the easy way to register community input on decisions like this. Please do more in the future!
- Thank you for allowing the public to give feedback on this very important project. I'll try to forward this along to friends/neighbors.
- Thank you for the opportunity to provide feedback! The bridge should serve the community of pedestrians, drivers, and bicyclers. The idea of the bridge serving as an architectural "wonder" (I'm stretching a bit here) or even just lighting the bridge so that the bridge itself stands out is not important to me at all. I'd rather funds go towards creating a useful bridge and surrounding space...it's of course preferable if it looks beautiful but function is the #1 concern for me.
- Lets be wise with the money and do what is most effective and not spend on too many unnecessary "artsy" things. We already have a "amphitheater" that stands out and is not with the Mall look.
- Please take a practical approach to re-doing the Belmont Bridge, in terms of moving people, cars, bikes, buses as the number one priority. Please do not devote the space and resources to parks, playgrounds and plazas. I like all of those spaces-- and have kids, so we enjoy these kinds of things-- but not in place of or as part of the Bridge.
- Helping to make a better connectivity between Belmont and downtown would be great. The bridge is used by so many people and is currently not very ped-friendly and has terrible aesthetics, design and upkeep surrounding its various ped approaches.
- I would like to see the bridge be multi-functional. With the amount of growth slated for the downtown area in the next 2-8 years, the bridge needs to work for the now and the future. Residential units and commercial spaces are both growing. The changes that were made in the Belmont area to beautify that area are aesthetically pleasing but have also taken away parking spaces and made it much more difficult for commercial vehicles to service those businesses. Downtown vehicle traffic seems to be the number one problem right now. If it wasn't, the city would not be purchasing Lucky 7 and Guadlajara to build another parking deck. This will add additional traffic to the Belmont Bridge. The design needs to take into account this growth.
- Please make sure you adequately address traffic and access issues, as opposed to favoring pedestrian and bike usage.
- Think long term!
- I walk and run across the bridge probably 20 times a month. Though I do not bike, I really hope there is a bike lane. When there is no bike lane, many bikers choose to bike on the sidewalks which is not safe for the biker or the pedestrians, particularly during times of heavy traffic. There is already quite a bit of open space on the mall, the lawn at the pavilion, lx park, etc. I don't see that as a high priority for this space. As a major thoroughfare connecting Belmont and downtown, I see the largest priority as improving traffic flow and safety for vehicles, bikers, and pedestrians. I would also really like to see both sides of the bridge open for pedestrian traffic. Having only one side open creates unnecessary

pedestrian street crossing which is likely the most dangerous part of a foot commute and also inherently slows vehicle traffic.

- Having some greenery and beauty when crossing the bridge would make it feel fun to walk between downtown and Belmont!
- I cross the bridge occasionally and live within 5 miles of the bridge.
- I ride a bike, but only infrequently across the bridge because it is not comfortable!
- I have made them in the comments.
- All depends where I'm working that day
- It seems we are making a simple need over complicated. We need a bridge primarily for car traffic but should of course also work for bikes & pedestrians. I think it should have been (re) built already.
- I am a daily bike commuter, but I never use the bridge because it is too scary for cyclists. I would use it if it felt safer for bikes.
- Just replace the bridge. Don't over think it. Keep it simple.
- Having a bike lane separate from pedestrians is important. A lot of people walk and bike this route, so please consider ADA friendly entrances and widths.
- I live off of Cherry Ave.
- I cross the bridge and the parking lot below often.
- The bridge is going to last a long time we should devote resources to it instead of taking the cheapest option. Let's have a space we can all enjoy instead of just something to move cars from point a to b. In the long run the extra cost will be worth.
- I live in Locust Grove neighborhood but walk and drive around town often
- We must continue to have parking options!!!
- I believe that the Belmont Bridge should first and foremost be functional. There are plenty of parks and recreational spaces in the area that residents can use. While I understand the importance of aesthetics, the bridge is an extremely high-traffic area and should be functional and help to reduce traffic downtown.
- I would probably bike and walk across the bridge more often if I felt safe doing so.
- Charlottesville is my hometown. I used to live in a Charlottesville near the Belmont Bridge. Now I commute to work from Waynesboro.
- The new bridge (or underpass) has the potential to be an iconic symbol, shaping the future of Cville. The new bridge should show that Cville is bike/ped friendly, environmentally sensitive, and places high importance on community spaces. It is a long term investment and our tax dollars are worth it. Do not underestimate the huge potential here. We need more park/open space downtown. I hope there is an opportunity to create more outdoor rec space with this project.
- Please keep the bridge simple. Charlottesville and the downtown mall already sells itself. The bridge doesn't need to be expensive nor magnificent. Save money and make this bridge as efficient as possible for bikers, pedestrians, and car traffic. Also, please preserve parking if you can!
- I own a home in Belmont.
- Park under it every day, walk over it every day, work across from it. Excited for this project

- I own my home on Little Graves Street, just a stone's throw from the bridge. In addition to a pedestrian walkway to and from downtown, a separate turning lane onto Graves is EXTREMELY important during high-traffic times. We are excited for a new and improved bridge, but want to make sure it suits those of us who live in and support the immediate neighborhood.
- Please stop trying to make this project about anything other than building a serviceable, utilitarian bridge. It's not a chance to design a park or a space that competes with the Mall. If you try to do that you will fail and you will waste a lot of public money while failing. Enough public money has already been wasted on this topic. Build a straightforward and useful bridge. That's all anyone has been commissioned to do.
- let's get this project underway. Make a decision and start. Not going to get total agreement ever.
- Downtown parking is always an issue. It would definitely be ideal to preserve any parking spots around the bridge
- Traffic seems to flow just fine with only one lane each way right now. It only seems to mildly bottleneck during rush hour and otherwise is used constantly by bikes and pedestrians. I would take that into consideration when balancing sidewalks and bike lanes versus car lanes.
- Make it functional and safe. Very little else matters.
- Make it functional. The approaches into and out of downtown are not large enough to accommodate the number of commuters. Make the bridge as modular as possible, especially considering you have to deal with the railroad.
- I live in Belmont and I am very concerned about how bridge construction will negatively impact our daily lives. Please be considerate of us and DON'T cut off pedestrian/bike access to downtown while the bridge is under construction.
- I work on Ivy Road and live on Rugby Avenue.
- I want wide wide wide wide sidewalks
- As a member of one of the connecting neighborhoods, this bridge is a gateway for me, not a destination. As such I'm more interested in seeing the transition elements, and integrating more bike/ped opportunities to really connect these currently pretty disparate parts of the city.
- It would be nice to connect the Belmont restaurant area to the downtown mall via this bridge design.
- Living in belmont, pedestrian access to the bridge is critical. There was a plan for an underpass that I quite liked, but I don't think it made it out of the design phase.
- 
- To me, the biggest problem with the current bridge (besides the ones that necessitate it being replaced to begin with) is that there is no access to it directly from the bus station. You can walk up and around by the pavilion, but this is NOT an option if there is a concert going -- the route is blocked and you need to go all the way up around the police station. I believe this is also the reason why so many people were using the railroad crossing through the fence until it was reinforced several times.
- 
- The bridge as it stands now serves more to make access to the mall more difficult rather than easier if there's any sort of ticketed event at the pavilion, and this can choke off pedestrian traffic to the belmont restaurant area during peak hours.
- Most importantly, I would like the bridge to be beautiful, and a "tourist draw." Secondly, I would like to retain the parking spaces underneath .. thx!

- I live in Belmont and work downtown. Most of the time I walk to work. When the ramp is closed for a pavilion event, this is very inconvenient, lengthening travel time with no warning until approaching the pavilion. I would like to see a bridge with pedestrian access to Water street or the east end of the downtown mall. I do not think the travel of private citizens who live and work downtown should be impacted by concerts, and other private events. I would like to see plants and trees along the bridge. I absolutely love the ginkgo trees on the ramp by the pavilion, for example, but the main purpose of the bridge should be to connect one place to another for cars, bikes and pedestrians. I do not care for expensive "iconic" design elements, or efforts to make the bridge any kind of park, event area or hang out, which will undoubtedly go unused. We just need a bridge that is safe. Thank you.
- I live in the Rosehill area. My primary mode of transportation is by bicycle.
- Belmont resident
- Used to live in Belmont and walked / biked the bridge daily. Now I drive daily across the bridge.
- Pedestrian use should include space for runners vs. walkers.
- make the bridge bike friendly
- would love to see the removal of traffic lights to access the bridge. Something like roundabouts would be ideal
- The time has come to replace the bridge.
- bike lanes throughout city would be nice. I have had police officer to regular drivers almost hit me or run me off roads
- Safe pedestrian walk with kids would be great. Barrier between Road and walking path would be ideal. Thanks for reaching out.
- At the 5th section - the progress bar does not appear to indicate that my survey input is 100% complete...yet there are not other options to add more information, continue or end the survey...so my only option is to close the browser to exit.
- I walk to and from work downtown from Belmont everyday. Pedestrians have no better way to get to the mall. When the pavilion closes off access it means a crazy detour, which on foot is hard. Please find a way to keep pedestrian access from Avon St to the mall open at ALL times.
- Pedestrian, bike, and vehicular access without train interruption is critical. Parking is already scarce and shouldn't be eliminated.
- Very cool survey! At end of day, looking for a bridge that is structurally sound and safe for everyone using it. Not as concerned with an iconic, beautifully decorated, bench-filled bridge with adjacent greenspace.
- This project has suffered from excessive overthinking of the problem. Solve the core issues and let the solution reinforce the good things going on around it. Stop trying to make it do more than is warranted or needed. Simple solutions should trump complex ones. It should be a connecting element in Charlottesville's urban design, not a destination or landmark.
- Could the walking/biking lanes be at grade versus the cars which could go over the train tracks? Could a pedestrian/bike path be a separate flyway from the car bridge?
- I have to both drive and walk across the bridge to get to work quickly. I hope that both pedestrians and drivers will be given a pleasant space to move around.
- My biggest issue as a citizen is having street furniture or park features on the bridge. It is really unfair to the Pavilion (as a business) and to the citizens who pay tickets to attend shows to put in more

places for folks to hang out for free during shows. Or alternately places for the vagrants to drink and sleep.

- Function for people and bikes is way more important than a fancy design although I don't think the two have to be exclusive of one another.
- The only reasons for this update should be basic functionality. If you want to add space for bikers fine. Traffic jams on this bridge are never bad, so don't use that as an excuse. There should be no sacrificing of existing parking, but if you can increase it, well then kudos to you. Any other reasoning thinking this will improve car traffic is flawed unless the construction was to expand all the way down Avon to Mill Creek, and up through the 9th street to High Street change over... which I don't think you are going to do since that would involve removing houses and businesses. If all you do is expand lanes in that small area you are just creating a larger potential for accidents and backups going from two lanes to four lanes then back to two. There's less room for error if everyone is already stuck in two lanes. Give them a small period of four lane freedom, and people will lose their minds.
- Be nice to have some shade on the bridge, during those hot summer days it feels like crossing a desert.
- I'm concerned about putting in large office buildings around the bridge. I live very close to the bridge in a neighborhood (intersection of Monticello and Graves) and I'm concerned that the neighborhood feel will be disrupted.
- Bridge design should not foreclose the opportunity, which may arise decades hence, to correct a basic flaw in the Pavillon. That space should have been multi-use design. It could have been the ideal place for the farmers market, for example. The new bridge should in no way hinder truck access via Water Street to the Pav. site.
- Walk and drive across the bridge everyday. I live in Belmont.
- Thanks for allowing this type of input from the community. Great idea!
- I use the bridge multiple times every day, often with different modes of transit. Reiterating a comment I made elsewhere in the survey -- this survey spends a lot of time on relatively superficial design elements of the bridge, such as public furniture, landscaping, fancy lighting, and so on.
- 
- These elements are nice to have, but getting the core utilitarian transit experience right for each of the three main modes of transit is \*much more important\*. People using all three modes of transit should be able to cross the bridge in a way that's comfortable, simple, and feels safe. Pedestrians, cyclists, and vehicles shouldn't have to compete with each other for space.
- I see the bridge from my back deck, am a regular bike commuter, and our neighborhood is filled with children between the ages of 2 and 9. Please pay close attention to the Graves crosswalk safety issues, as well as ensuring that the ped, and if possible, bike facilities are protected, and please include ramp access down to Water Street, or at least stairs. Bonus points if you can get the railroad to allow a bike/ped at-grade crossing just East of the bridge.
- My family lives in the neighborhoods near the bridge also.
- Ideally, the bridge has the shortest span possible to better connect downtown to Belmont, allowing for more occupiable, shaded, open space. Ideally, cars are separated from bikes and pedestrians to allow safe, comfortable travel across the bridge) without overbuilding the bridge (and making less usable space beneath).
- My office is in the shadow of the bridge, I don't want to lose my space and I want this bridge practical forget the style pedestrians and bikes are more important than cars

- Let's remember that Belmont is a residential neighborhood with lots of young families. But that you are also dealing with an increase in traffic/parking needs along this corridor. And let's also please MOVE THINGS ALONG. The Bridge is unsafe and needs to be replaced ASAP.
- I walk under the bridge twice a day to and from work
- This is the coolest survey I have ever seen. I was educated!!! Thank you! I hope the City will continue to use awesome technologies like this!
- I come from Crozet and as a commuter I still hold the value of the bridge to pedestrians and cyclists as higher than that of motorists. There are lots of folks who walk the bridge from downtown to visit Belmont etc. so it should accommodate pedestrians coming from the garages.
- this project is very important to me because the bridge has caused me to have concern for my safety as a pedestrian. thanks for asking for my input.
- This bridge has to be 4 lanes. Deliberately restricting access to downtown is insane.
- This is an important project and I appreciate that thought is going into it to take into account not only functional needs but to have the bridge reflect the kind of city that Charlottesville is and aspires to be.
- As a taxpayer, I want to manage costs for projects like these. Maybe use some of the money earmarked for removal of the R.E. Lee statue for a project like this which is structurally necessary.
- Would frequent that part of town more if it were safer and more pleasant to walk and bike to get there.
- Just build a not ugly bridge with some sidewalks and bike lanes and don't clog up the traffic..this is not rocket science charlottesville...no one else seems to have trouble doing these things.
- I live about a 25 minute walk away. I drive it nearly daily, and only walk it about 1-2 month. (I would walk more frequently but I don't feel too safe at night once I get to about where Browns is.
- Bridge needs to be at least 4 lanes and pls preserve parking.
- Grew up near the bridge, still own property near the bridge
- A world-class city with an eyesore that is falling apart as one of main entry corridors. What a shame. This should be DONE not up for redesign.
- I walk and drive a crossed the bridge daily. Traffic is really bad from 430 to 6. Need 2 lanes each way.
- I would like a beautiful bridge to experience both as a pedestrian and a driver. Beautiful bridges enhance cities.
- I used to live in the neighborhood and still walk to friends' houses.
- If you really want to enhance the area - the road should go under the train tracks.
- Don't take away vehicle capacity. It will cause traffic jams on this highly used bridge.
- Pedestrian access is important, but there is a lot of traffic on the bridge and reducing the number of lanes would make the situation worse. Please make sure the bridge is well-designed for cars before worrying about aesthetics.
- Given the "look" that is developing with the Pavilion and other buildings, I think a strong, complimentary aesthetic is important. Not doing do is like not springing for the floor mats in a new car. \$400 made sense at the dealership but you regret it for a long time.
- Please preserve the views from the bridge, make bike and pedestrian friendly.

- These questions seem all over the place and I cannot imagine they will yield any useful information when all the city residents really need is a functional bridge that you can drive, walk, or bike across. If you read the comments on NBC29's website for any story about the bridge replacement project you will see the frustration.....and this is why.
- Please just build it and stop all the surveys and case studies.
- Traffic tends to get very backed up on and near the Belmont Bridge during rush hour, which can be frustrating. It would be nice if this could be addressed in the new bridge design. However, I would also love to see more greenery on and around the bridge (planted median and sidewalks, green space underneath bridge, etc.). Thanks!
- I am greatly concerned about losing any parking. Before you do that I would suggest you first find replacement parking . Otherwise, it should not be an option for this project
- The concerns I have surround the actual construction project. As we learned from the JPA bridge replacement the railway is a Huge factor. Owing to regs (requiring removal of construction equipment, before and after each train pass) the total "down" time will be extended at least five fold. I fear the volume of traffic this conduit channels is being under-estimated. The community can't handle a lengthy down time- the two other avenues cannot adequately handle the overload. This is demonstrated by the existing traffic back-ups at the 2 major entrance ways to the downtown area from the south. The selected project plan must require at least one lane in each direction be open at all times, throughout the project.
- Whereas a huge cross section of local people drive across the bridge each day, only the increasingly yuppified Belmont neighborhood residents walk across the bridge. I am all for walking, but the folks who live near there can easily flood your survey with their opinions because they stand to gain so much. The traffic flow is the most important thing if you are being democratic and honest.
- The intersection at Avon and Levy has too many incoming streets. Close old Avon and move that access to 6th street and around. I have always envisioned using the open area on west side for commercial buildings with cafes fronting a wide bridge-side sidewalk and public parking behind these buildings.
- Take the money from the ridiculous removal of war memorials and use it for the new bridge.
- I don't live near the bridge, but am concerned with the lack of pedestrian safety on the streets.
- Get it built and don't worry so much about making it "pretty" or creating new spaces to play or have activities. There are MANY other projects that the city needs to fund. It's a bridge; don't spend money to make it "art"
- The Belmont Bridge icon evokes a contemporary image. I hope the design of the bridge resemble the icon
- I walk a few times a month.
- I'd like to see a modern urban condition: concrete and steel, designed for heavy pedestrian use side-by-side w/cars and bikes.
- one lane each way is a bad idea
- Would bike across if there were dedicated lanes.
- Please add a design for at least 4 lanes of traffic. The city will continue to grow as will traffic. Adding only 2 traffic lanes will mean more backed up traffic, as there currently is.
- Also consider having traffic tunnel under the train tracks, or have the above ground bridge have an iconic design.

- Looking forward to seeing what design ideas will get presented in public meetings. The budget amount for the bridge (\$23mm??) would seem to preclude much in the way of innovative design. It's likely Cville will get what it can pay for, and that's likely to be not much different from what's there now.
- This survey was frustrating. I wasn't sure if I was ranking typologies or voting on the example shown. I think there should be a tunnel for cars with bike and pedestrian paths on top. Planters and median plantings are too labor intensive... choose street tree cultivars that thrive in urban high ph environments for shade and beauty... if you do a bridge maximize the shade under the bridge somehow .. extend the social space of the pavilion... extend the farmer's market there. Baltimore's farmer's market under the overpass is fantastic in the heat of summer.
- The City needs to preserve as many of the 53 public parking spaces as possible while making it more aesthetically attractive. Both are possibl. Thanks
- This bridge is a ridiculous design solution. The underpass is the correct way to proceed. As a design professional, I am looking for the adults to step up and compel a different solution. The price of the bridge is now almost exactly the cost of the underpass.
- It MUST connect to water street easily.
- Shocking that this project has taken so long to get done.
- I am very concerned about parking and traffic needs downtown. We are outgrowing our roadways. The proper balance must be struck between pedestrians/cyclists and vehicles.
- I utilize the bridge every day both driving and walking. Please preserve these functions and please do not eliminate any of the parking.
- I would like to keep the parking beneath the bridge and the sidewalks to walk to work efficient.
- Traffic across the current bridge is terrible. Never have I seen a bike crossing it. It needs to be of a design that is easy to maintain as the City does not have a good track record of maintaining its bridges.
- As one of the only ways for traffic to get from the north side of town to the south side of town I hope the project outcome will not make it more difficult for vehicular traffic. With more cars and a new parking structure right at the end of the bridge, I hope we won't ignore the fact that one of the main purposes of the bridge is to efficiently move traffic.
- Please don't take away my free parking.
- Please keep parking under the bridge a top priority!!
- I do not live in Belmont anymore, but I used to, and I'm fearful for the pedestrians, including those with children on the bridge as it is today. We need to ensure there's plenty of room and buffer space for our young families walking across the bridge.
- Bridge needs to accommodate bike, ADA, pedestrian and vehicular traffic in a very safe manner for all users.
- Build a bridge with at least the current traffic carrying capacity. Otherwise, it will be misery for those living nearby.
- Keep one lane open each way at all times (except occasional nighttime) during construction.
- I also run and walk my dog across this bridge daily. Appreciate any efforts to make it pedestrian friendly!
- Please try to make this thing work. Give us a nice wide bridge that spans as far as this one does and keeps the views. Give us a separated bike/walk path and keep the fancy seating/ public areas to a min. We want a bridge that gets us from place to place safely and keeps the nice views and functions.

Cville will try as they always do to waste time and money giving the bridge its own wifi and making lots of amazing "space" for the bums to sleep and leave cigarette butts and fly signs. Resist the urge to make a bridge into a playground or a venue. Think Brooklyn Bridge-- works for transpo gives views functions way better than anything anyone in Cville has ever come up with. You can do it!!! Also put some money aside so they can maintain it. The fact that they let this bridge fall so far into disrepair is just another testament to the inability of the stumbling well meaning incompetents who run our little village. Thanks and good luck.

- I am torn between having more green space under the bridge and taking away the parking. However, parking is a big problem in the downtown area and if that is remedied then I would say go for the green space.
- Minimize seating to reduce street people.
- Connection to downtown is crucial.
- We are a one car family and I ride my bike over the bridge (currently feel's a bit intimidating to do this). Please make the bridge more pedestrian and bicycle friendly!
- As someone who works downtown and is familiar with the traffic patterns...getting rid of the four lanes on the bridge would be a disaster during rush hour. The amount of bike and pedestrian traffic truly does not require 6 foot pedestrian AND bike lanes! I cross the bridge often, and the amount of people that I see on there only increased after the other side of the sidewalk was closed off \*years\* ago.
- I would personally appreciate it if the bridge was prettier, but it's not a priority at this point! I (and many other people) just want it to work!!
- Whether or not one lives near the Belmont Bridge, it is an obvious gateway to Cville's Downtown and should be made as attractive or welcoming as possible. Vehicular traffic is likely to remain the prime user of the bridge area, so good sightlines will be vital, as will clearly signed directions. But appealing sights to look at all around the bridge and its approaches also will be appreciated at 25 mph, as at a 2 mph stroll.